

| TURNOVER RATE OF CAR PARKING AREA (a) | TYPE OF FRONTAGE ROAD | TYPE OF DRIVEWAY FOR THE NUMBER OF SPACES IN CAR PARKING AREA |        |         |              | NOTE  |
|---------------------------------------|-----------------------|---|--------|---------|--------------|---|
|                                       |                       | 1-25  | 26-250 | 251-500 | OVER 500 (b) |   |
| LOW/MED                               | MINOR                 | A (c)   | B2     | C1      | C3           | a) LOW TO MEDIUM PARKING TURNOVER RATES ARE LIKELY TO BE GENERATED BY RESIDENTIAL, INDUSTRIAL AND COMMERCIAL DEVELOPMENTS. HIGH PARKING TURNOVER RATES ARE LIKELY TO BE GENERATED BY ENTERTAINMENT, TRANSPORT, RETAIL AND FAST FOOD DEVELOPMENTS. |
| LOW/MED                               | MAJOR                 | B1 (6m)   | C1     | C2      | C3           | b) CAR PARKING AREAS CONTAINING OVER 500 SPACES OR GENERATING MORE THAN 1,000vpd ARE TO BE ASSESSED FOR THE NEED OF AN APPROPRIATELY DESIGNED CHANNELISED ACCESS INTERSECTION.  |
| HIGH                                  | MINOR                 | B1 (7m)   | C1     | C2      | C3           | c) ON MINOR ROADS, RESIDENTIAL (TYPE A) DRIVEWAYS LESS THAN 6m WIDE ARE ACCEPTABLE FOR STREETScape ENHANCEMENT, PROVIDED NORMAL MANOEUVRING AND QUEUING REQUIREMENTS ARE SATISFIED  |
| HIGH                                  | MAJOR                 | B2 (7m)   | C2     | C3      | C3           |   |

| FRONTAGE ROAD                              | MINOR ROAD <100vpd | MAJOR ROAD | NOTE  |
|--|--------------------|------------|---|
| NOMINATED DESIGN VEHICLE (d) DRIVEWAY TYPE |                    |            |   |
| CAR AND TRAILER                            | A (6m)             | C1         | M   |
| SERVICE VEHICLE 8.8m                       | B2 (7m)            | C2         | PATHS OF THE VEHICLES IS REQUIRED TO BE SUBMITTED TO COUNCIL TO DEMONSTRATE HOW THE VEHICLE WILL PRACTICALLY ACCESS THE PROPERTY. |
| SINGLE UNIT TRUCK 12.5m                    | B2 (7m)            | C2         | ACCESS FOR SUCH VEHICLES REQUIRE FORWARD ONLY MANOEUVRE FOR ENTRY AND EXIT OF THE PROPERTY.                                       |
| REFUSE COLLECTION VEHICLE                  | B2 (7m)            | C2         |   |
| BUS  | B2 (9m)            | C4         |   |
| PRIME MOVER                                | B2 (9m)            | C4         |   |
| B-DOUBLE                                   | B2 (9m)            | C4         |   |

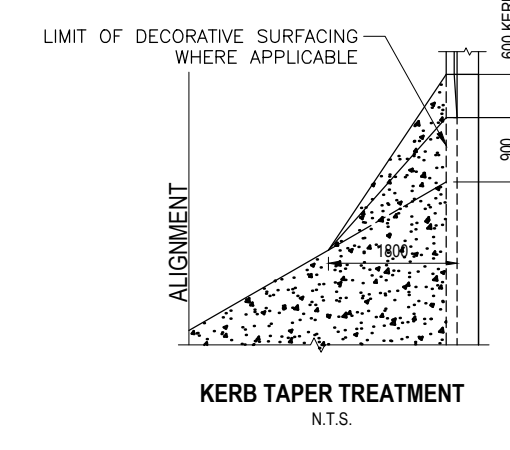
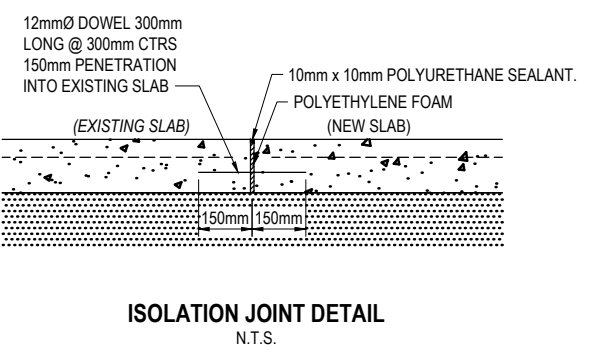
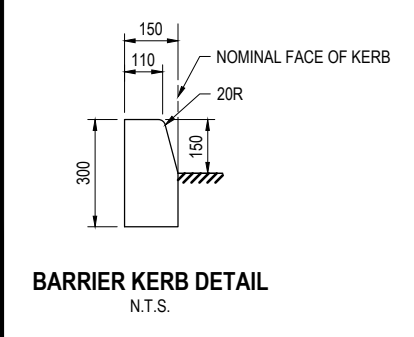
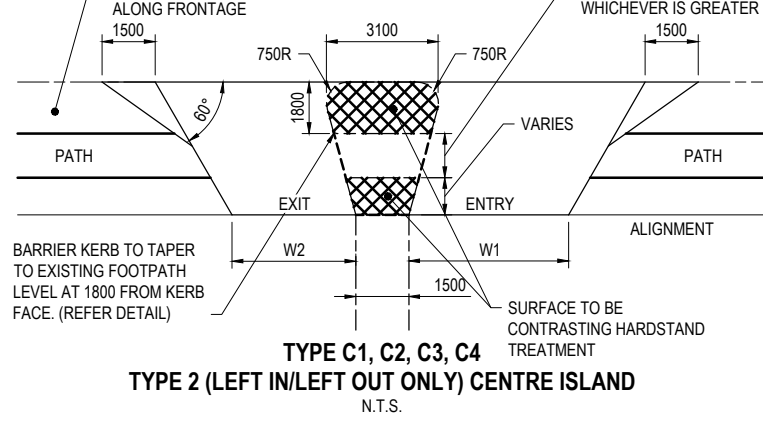
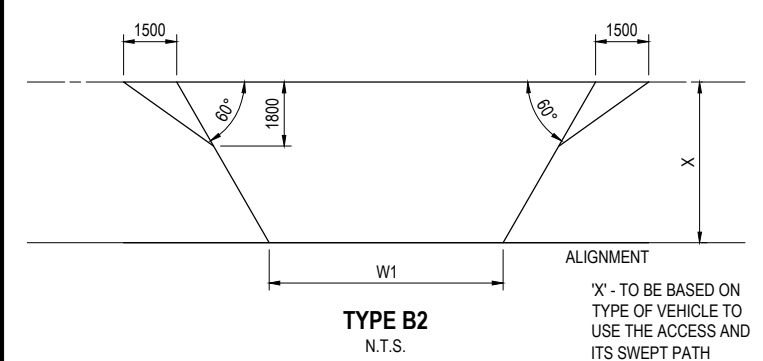
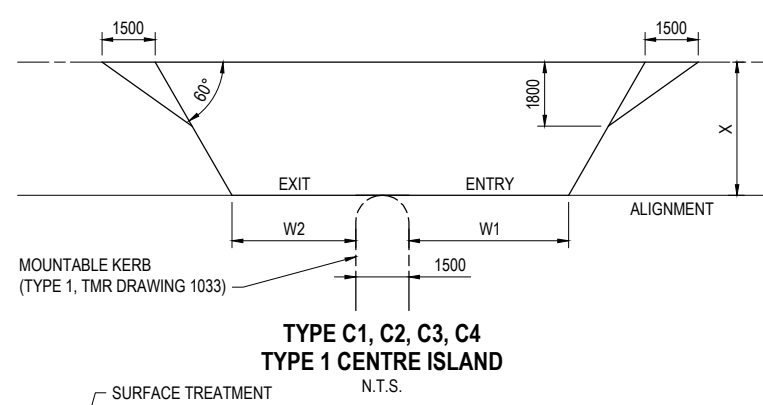
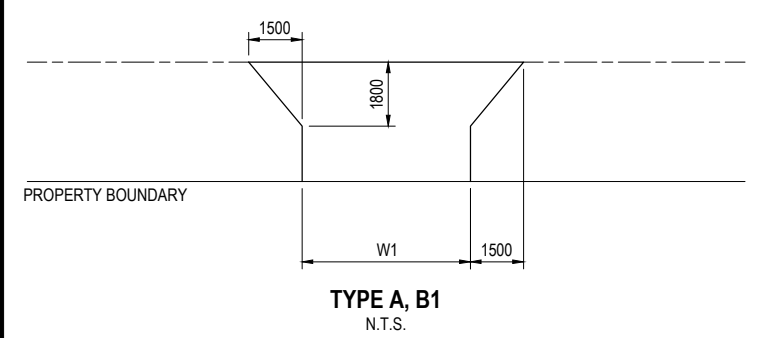
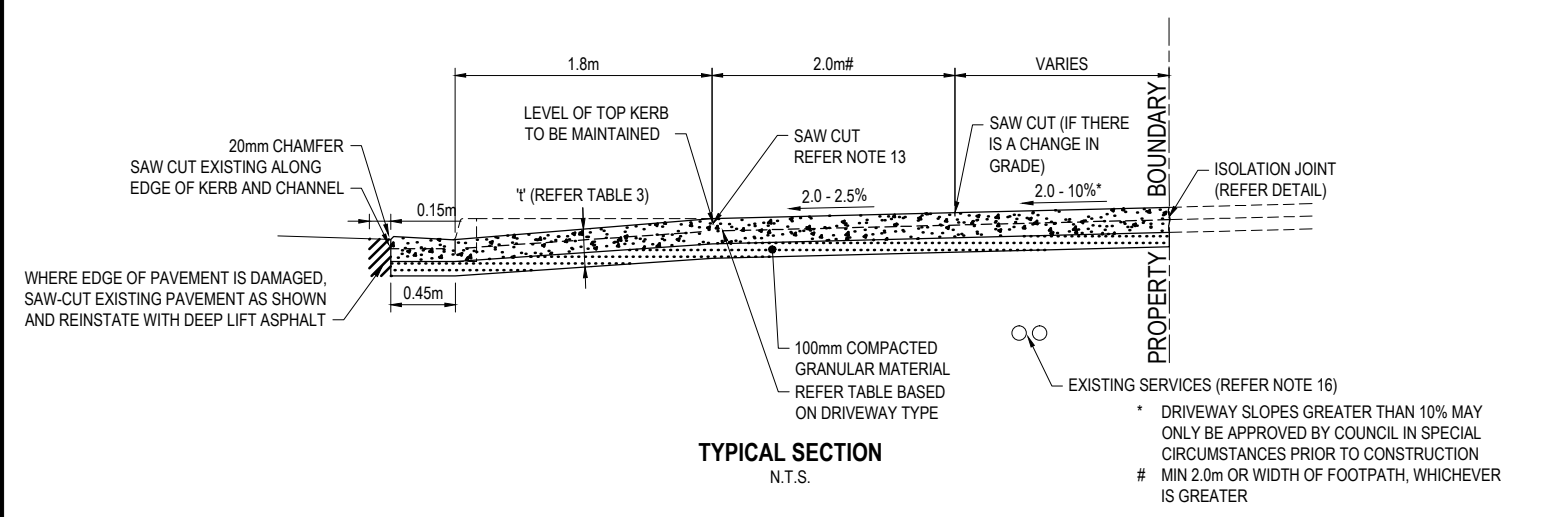
- RELEVANT STANDARDS:**
- AS3600, CONCRETE STRUCTURES
  - AS1379, SPECIFICATIONS AND SUPPLY OF CONCRETE
  - AS/NZS A4671, STEEL REINFORCING MATERIALS.

- NOTES:**
- ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE STATED.
  - CROSSING TYPE, LOCATION AND IF RELEVANT, DIMENSIONS W1 AND W2 WILL BE DETERMINED BY COUNCIL.
  - NEW FOOTPATH PROFILE TO VARY WHERE NECESSARY TO MATCH WITH EXISTING CONCRETE FOOTPATHS AND VERGE PROFILES. BASE LAYER MUST BE WELL COMPACTED AND TRANSITION SMOOTHLY TO AND FROM DRIVEWAY. ADJOINING FOOTPATH SURFACES SHALL NOT HAVE A GRADE GREATER THAN 1:8 OR 12.5%.
  - PEDESTRIAN REFUGE AREA TREATMENT TO MATCH TREATMENT ADJACENT TO CROSSING.
  - NO TRACKS PERMITTED ACROSS FOOTPATH.
  - CONCRETE PATH SHALL BE CONTINUOUS ACROSS DRIVEWAYS AS PEDESTRIAN AND CYCLISTS SHOULD HAVE RIGHT OF WAY.
  - VARIATION TO THE DESIGNS SHOWN ARE SUBJECT TO APPROVAL FROM THE MANAGER ROAD SERVICES OR THIS DELEGATE.
  - CONCRETE GRADE N32 OR BETTER; REINFORCEMENT AS PER TABLE, MIN LAP 210 MIN, MIN CLEAR TOP COVER 50mm.
  - ALL VERTICAL FACES ARE TO BE FORMED, INCLUDING THE INTERFACE WITH THE ROADWAY.
  - FORMWORK AND REINFORCEMENT MUST BE IN PLACE AND INSPECTED AND APPROVED BY COUNCIL OFFICER BEFORE DELIVERY OF THE CONCRETE.
  - THE THICKNESS OF DECORATIVE SURFACING WHERE APPROVED IS ADDITIONAL TO THE THICKNESS DIMENSIONS 't' SHOWN IN TABLE.
  - BRASS DISKS EMBEDDED IN KERB AND CHANNEL SHALL NOT BE REMOVED WITH OUT THE PERMISSION OF COUNCIL.
  - SAW CUT TO BE 3-6mm WIDE x  $\frac{D}{2}$ , WHERE D = DEPTH OF PAVEMENT. APPLY BEAD OF POLYSULPHIDE SEALANT TO BOND BREAKING TAPE.
  - FINISH: WOOD FLOAT OR STEEL FOLLOWED BY NYLON BROOM OR OTHER APPROVED NON-SLIP SURFACE.
  - SURFACE TREATMENT MAY NOT BE MATCHED BY COUNCIL IF REPAIRS ARE NEEDED TO SERVICES UNDER DRIVEWAY.
  - ENSURE MIN. COVER TO SERVICES IN FOOTPATH AND DRIVEWAY IS ACHIEVED TO MEET RELEVANT AUTHORITY STANDARD. A DIAL BEFORE YOU DIG (DBYD) IS TO BE UNDERTAKEN PRIOR TO COMMENCING WORK ON SITE.

| TYPE | W1 (m)    | W2 (m) | t (mm) | REINFORCEMENT MESH |
|------|-----------|--------|--------|--------------------|
| A    | 6.0       | -      | 130    | SL82               |
| B1   | 6.0       | -      | 180    | SL92               |
| B2   | 6.0 - 9.0 | -      | 180    | SL92               |
| C1   | 4.5       | 3.5    | 180    | SL92               |
| C2   | 5.5       | 5.0    | 180    | SL92               |
| C3   | 7.5       | 6.0    | 180    | SL92               |
| C4   | 9.0       | 7.5    | 180    | SL92               |

- ALL WATER VALVES, HYDRANTS, SEWER MANHOLE, TELECOMMUNICATION PITS AND THE LIKE TO BE RELOCATED CLEAR OF PROPERTY ACCESS AT THE EXPENSE OF THE PROPERTY OWNER. THE RELEVANT AUTHORITY IS TO BE CONTACTED SO THAT CONFLICTING SERVICES CAN BE RELOCATED PRIOR TO CROSS OVER CONSTRUCTION
- THE PROPERTY OWNER / APPLICANT / CONTRACTOR IS TO TAKE ALL NECESSARY MEASURES TO ENSURE PEDESTRIAN SAFETY INCLUDING BUT NOT LIMITED TO BARRICADES, SAFETY LIGHTING, WARNING DEVICES OR OTHER MEANS OF PROTECTING PUBLIC RISK IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- WHERE NEW CONCRETE WORKS ABUTS EXISTING CONCRETE WORK, 12Ø DOWELS 300mm LENGTH (500mm LENGTH AT INVERT OF KERB AND CHANNEL) AT 300mm CENTRES TO BE INSTALLED TO PREVENT DIFFERENTIAL MOVEMENT (REFER ISOLATION JOINT DETAIL).
- COUNCIL TAKES NO RESPONSIBILITY FOR A VEHICLE SCRAPING WHEN USING A FOOTPATH CROSSOVER OR INVERT CROSSING. THE PROPERTY OWNER/APPLICANT/CONTRACTOR IS TO ENSURE ADEQUATE VEHICLE CLEARANCE IS PROVIDED.
- MANDATORY COUNCIL INSPECTIONS ARE REQUIRED PRIOR TO CONSTRUCTION INCLUDING CONCRETE SLAB SET-UP AND REINFORCEMENT, AND FINAL INSPECTION FOLLOWING COMPLETION OF CONSTRUCTION, INCLUDING BACK FILLING TO EDGE AND ENSURING THE NEW DRIVEWAY WILL NOT CAUSE A TRIPPING HAZARD.
- AN APPLICATION TO "CARRY OUT WORKS ON A COUNCIL ROAD" IS TO BE SUBMITTED BEFORE WORKS ARE UNDERTAKEN.

|                | BSC        | CHRC | GRC | IRC | LSC | MRC | RRC |
|----------------|------------|------|-----|-----|-----|-----|-----|
| Applicable     | No         | No   | Yes | No  | No  | No  | No  |
| Applicable DWG | CMDG-R-042 |      |     |     |     |     |     |



FOR RURAL COMMERCIAL AND INDUSTRIAL ACCESS, REFER COUNCIL'S STANDARD DRAWINGS FOR MAJOR ACCESS POINTS

| REVISIONS             | DATE    |
|-----------------------|---------|
| B IRC ADDED           | 12/2016 |
| A NEW DRAWING FOR GRC | 04/2016 |

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**Capricorn Municipal Development Guidelines**  
Incorporating:  
Banana Shire Council (BSC)  
Central Highlands Regional Council (CHRC)  
Gladstone Regional Council (GRC)  
Isaac Regional Council (IRC)  
Livingstone Shire Council (LSC)  
Maranoa Regional Council (MRC)  
Rockhampton Regional Council (RRC)

**URBAN COMMERCIAL/INDUSTRIAL DRIVEWAY**

ROADS  
STANDARD DRAWING  
CMDG-R-042A  
REV. A B