

NOTES

1. GRC MINIMUM GUIDELINES - IN ACCORDANCE WITH CURRENT
- DEPARTMENT OF TRANSPORT AND MAIN ROADS TRANSLINK DIVISION - PUBLIC TRANSPORT INFRASTRUCTURE MANUAL AND DISABILITY STANDARDS FOR ACCESSIBLE PUBLIC TRANSPORT (DASPT).
 - THE DISABILITY DISCRIMINATION ACT 1992 AND AS/NZS 1428.4.1 - DESIGN FOR ACCESS AND MOBILITY - MEANS TO ASSIST THE ORIENTATION OF PEOPLE WITH VISION IMPAIRMENT - TACTILE GROUND SURFACE INDICATORS (TGSi).

HARDSTAND

2. THE PREFERRED MINIMUM HARD STAND AREA IS 3850MM X 2400MM. A LARGER HARDSTAND AREA IS PREFERRED AND DEPENDENT ON SITE SPECIFIC CONDITIONS.
3. THE MANDATORY (COMPLIANT TO DSAPT) MINIMUM BOARDING POINT HARD STAND AREA IS 1540MM X 2070MM (SHOWN HATCHED) AND SHALL BE CLEAR OF HAZARDS.
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ACCESS

6. WHERE BUS STOPS ARE LOCATED ALONG BICYCLE ROUTES SHARED ACCESS PATHS SHOULD BE APPLIED AS PER LOCAL AUTHORITY REQUIREMENT.
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- CB3924-10101310-01.
- CB3924-10101310-02.
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- 3 ARM RESTS
- BOLTED TO THE SHELTER SLAB
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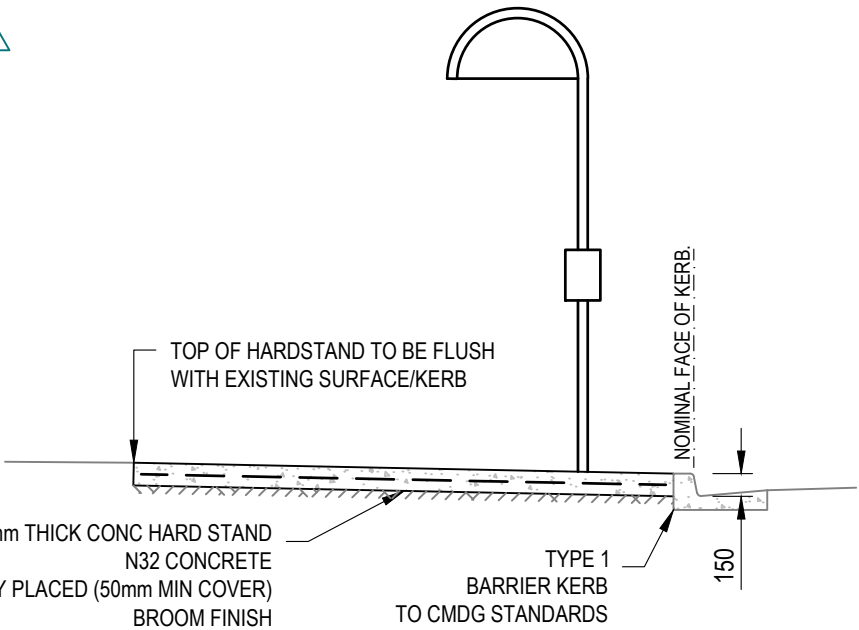
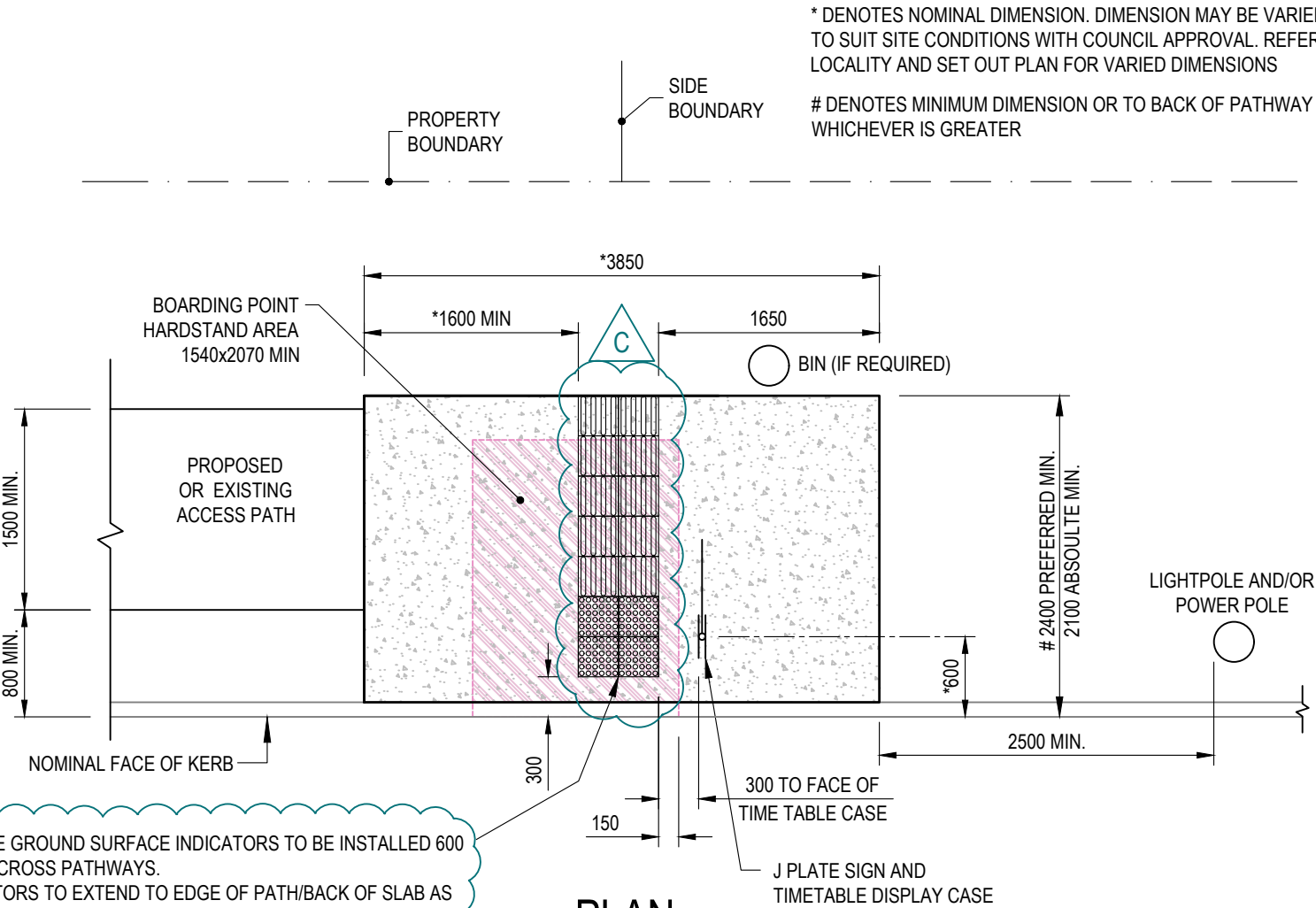
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TYPICAL SIDE ELEVATION

SURVEY REF: COORDS: AZIMUTH DATUM: MGA ZONE 56 LEVEL DATUM: AHDD	AMENDMENTS		SCALES						GLADSTONE REGIONAL COUNCIL  5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld. 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au			GRC BUS STOP STANDARDS BUS STOP- MINIMUM BOARDING POINT DETAIL PLAN				CONSTRUCTION				
	REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE														
SURVEYED	A	CONSTRUCTION ISSUE	DJC				DESIGNED	D.J.CAMPBELL	JUN 2014	 5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld. 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au	GRC BUS STOP STANDARDS BUS STOP- MINIMUM BOARDING POINT DETAIL PLAN				DRAWING NO. GRC-B-001					
	B	TRANS LINK AMENDMENTS	DJC				DRAWN	D.J.CAMPBELL	JUN 2014											
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							REPQ No	04340	OCT 2014											
							RPEQ	IAN MUNRO*			GRC BUS STOP STANDARDS BUS STOP- MINIMUM BOARDING POINT DETAIL PLAN				A3 SHEET		REVISION		C	

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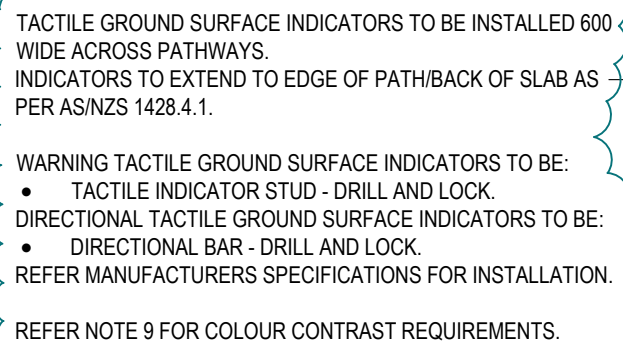
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The drawing consists of two parts. The main part is a 'TYPICAL SIDE ELEVATION' of a kerb. It shows a kerb with a height of 1200 mm. The kerb is made of concrete and has a width of 2400 mm preferred minimum, 2100 mm absolute minimum. A lamp post is shown mounted on the kerb. A callout points to the 'KERB DETAIL' which shows a cross-section of the kerb. The kerb has a width of 150 mm and a height of 200 mm. The top surface is sloped at 1:4. The kerb is shown with a 40 mm radius (R20) at the top corner. The kerb is shown with a 150 mm width and a 200 mm height. The kerb is shown with a 40 mm radius (R20) at the top corner. The kerb is shown with a 150 mm width and a 200 mm height. The kerb is shown with a 40 mm radius (R20) at the top corner.

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		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	DESIGNED								
SURVEYED		A	CONSTRUCTION ISSUE	DJC				D.J.CAMPBELL	JUN 2014							
		B	TRANSLINK AMENDMENTS	DJC				D.J.CAMPBELL	JUN 2014							
		C	AMENDED TO SUIT PTIM 2015	MAB	SGM*	JEK*	32/12/2016	REPQ No.	04340	OCT 2014						
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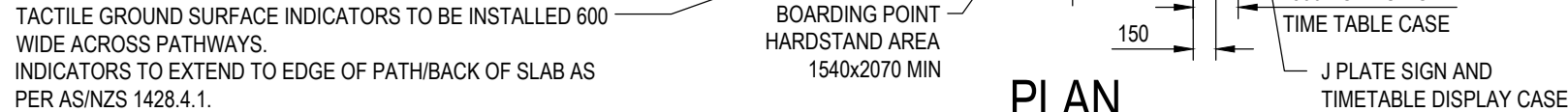
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WARNING TACTILE GROUND SURFACE INDICATORS TO BE:

- TACTILE INDICATOR STUD - DRILL AND LOCK.

DIRECTIONAL TACTILE GROUND SURFACE INDICATORS TO BE:

- DIRECTIONAL BAR - DRILL AND LOCK.


REFER MANUFACTURERS SPECIFICATIONS FOR INSTALLATION.

REFER NOTE 9 FOR COLOUR CONTRAST REQUIREMENTS.

A EACH DEDICATED SPACE SHALL BE IDENTIFIED BY MEANS OF A WHITE SYMBOL OF ACCESS IN ACCORDANCE WITH AS/NZS 1428.1 BETWEEN 600 MM AND 700 MM HIGH PLACED ON A BLUE RECTANGLE WITH NO SIDE MORE THAN 800 MM, PLACED AS A PAVEMENT PARKING.

* DENOTES NOMINAL DIMENSION. DIMENSION MAY BE VARIED TO SUIT SITE CONDITIONS WITH COUNCIL APPROVAL. REFER TO LOCALITY AND SET OUT PLAN FOR VARIED DIMENSIONS



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		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	DESIGNED	D.J.CAMPBELL	JUN 2014						DRAWING NO.	
A	CONSTRUCTION ISSUE	DJC				DRAWN	D.J.CAMPBELL	JUN 2014	GRC-B-003								
B	TRANSLINK AMENDMENTS	DJC															
C	AMENDED TO SUIT PTIM 2015	MAB	SGM*	JEK*	02/12/2016	VERIFIED											
SURVEYED		D	GOSSI PARK DRAWING NUMBERS REVISED	AF	SGM*	HZ*	15/11/2019	REPQ No.	04340	OCT 2014							
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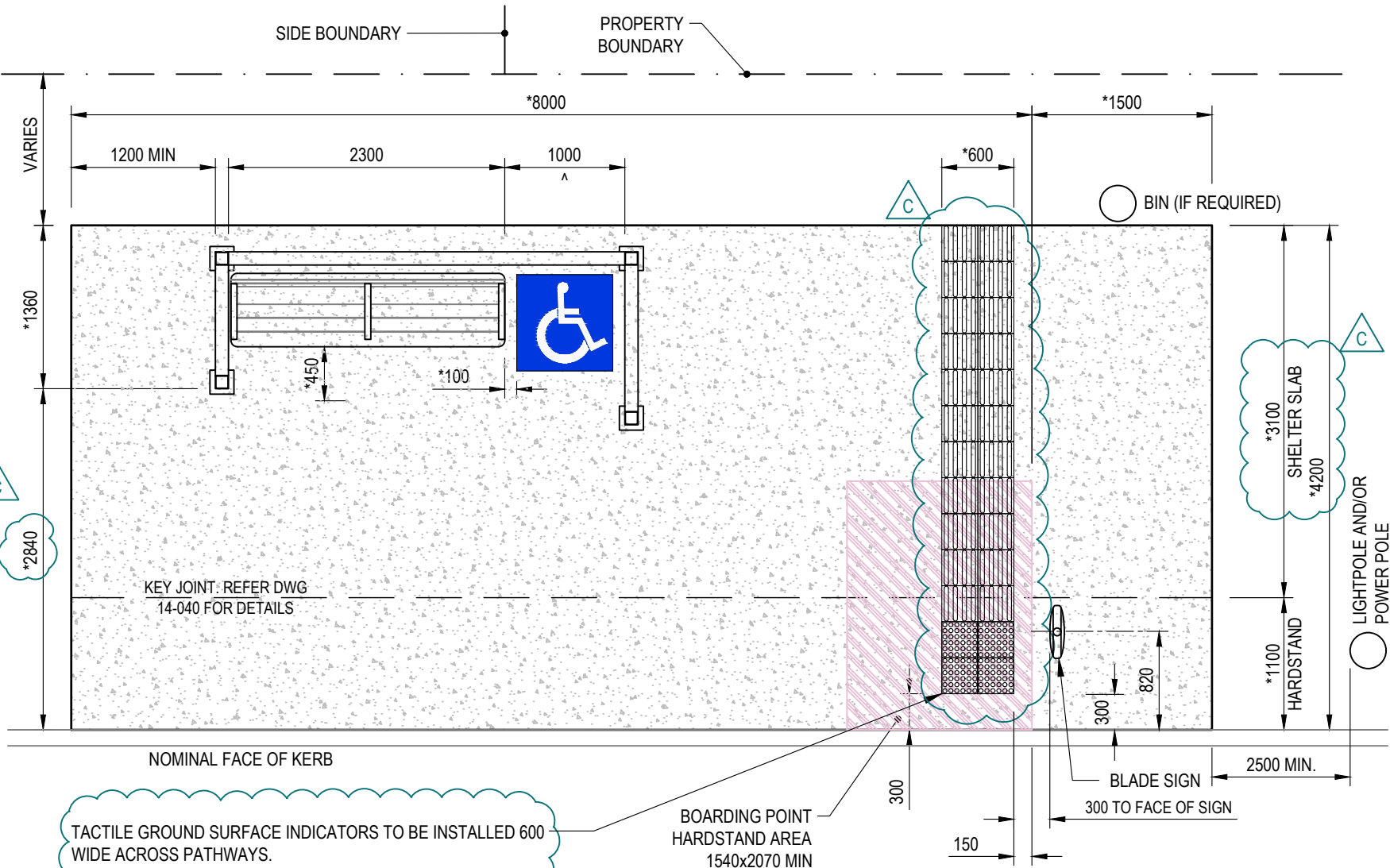
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16. WHERE PLANTINGS ARE PROVIDED, USE ONLY GROUND COVER OR LOW SHRUBS (<0.5m HIGH). TREES FOR SHADE SHOULD BE LONG-TRUNKED WITH MINIMUM BRANCH HEIGHT OF 4.5m. PLANTINGS SHOULD NOT OBSTRUCT LINE OF SIGHT BETWEEN APPROACHING BUS AND WAITING PASSENGERS. VEGETATION SHOULD BE CLEAR OF ANY EXISTING SERVICES IN THE AREA.



TACTILE GROUND SURFACE INDICATORS TO BE INSTALLED 600 WIDE ACROSS PATHWAYS. INDICATORS TO EXTEND TO EDGE OF PATH/BACK OF SLAB AS PER AS/NZS 1428.4.1.

WARNING TACTILE GROUND SURFACE INDICATORS TO BE:

- TACTILE INDICATOR STUD - DRILL AND LOCK.

DIRECTIONAL TACTILE GROUND SURFACE INDICATORS TO BE:

- DIRECTIONAL BAR - DRILL AND LOCK.

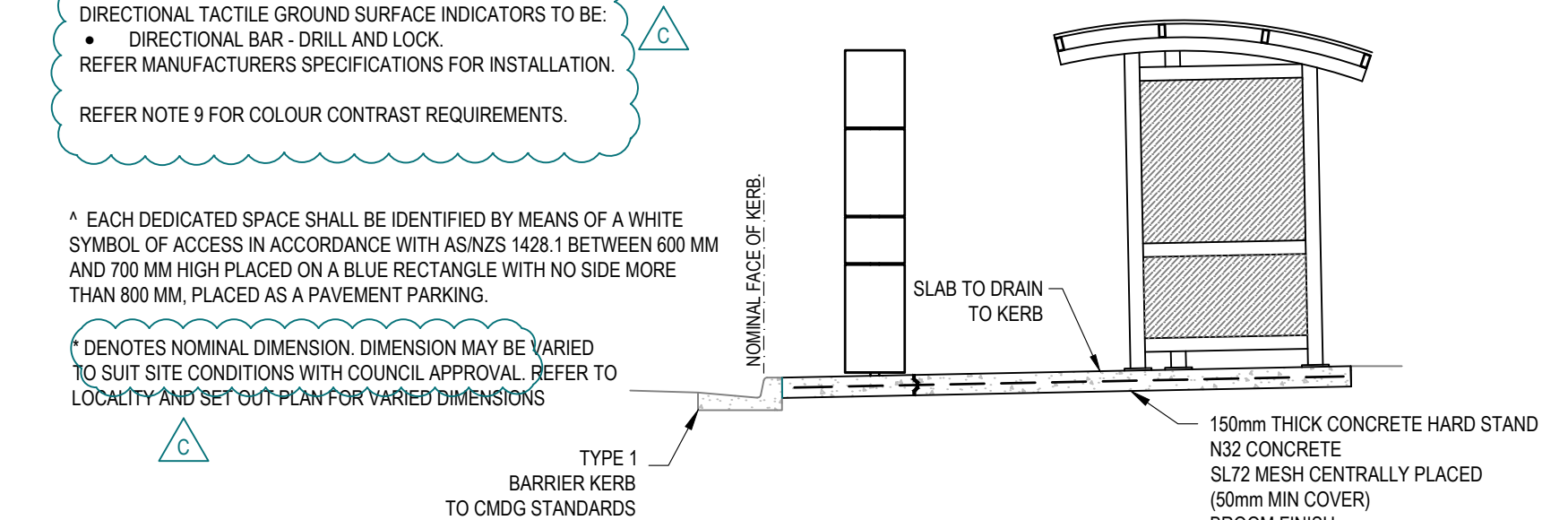
REFER MANUFACTURERS SPECIFICATIONS FOR INSTALLATION.

REFER NOTE 9 FOR COLOUR CONTRAST REQUIREMENTS.

^ EACH DEDICATED SPACE SHALL BE IDENTIFIED BY MEANS OF A WHITE SYMBOL OF ACCESS IN ACCORDANCE WITH AS/NZS 1428.1 BETWEEN 600 MM AND 700 MM HIGH PLACED ON A BLUE RECTANGLE WITH NO SIDE MORE THAN 800 MM, PLACED AS A PAVEMENT PARKING.

* DENOTES NOMINAL DIMENSION. DIMENSION MAY BE VARIED TO SUIT SITE CONDITIONS WITH COUNCIL APPROVAL. REFER TO LOCALITY AND SET OUT PLAN FOR VARIED DIMENSIONS

PLAN



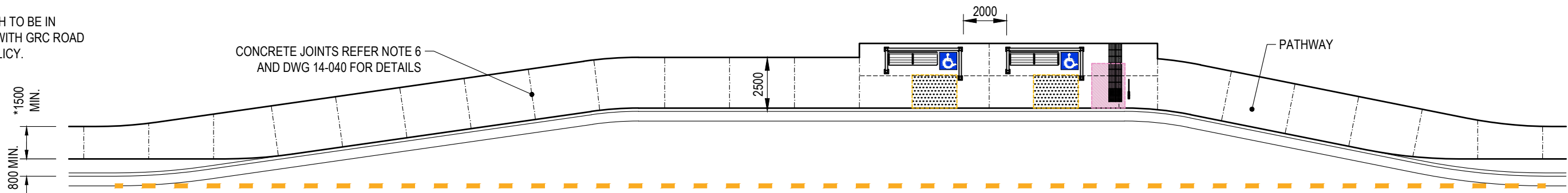
TYPICAL SIDE ELEVATION

SURVEY REF: COORDS: AZIMUTH DATUM: MGA ZONE 56 LEVEL DATUM: AHDD	RL:	AMENDMENTS					SCALES		DESIGNED D.J.CAMPBELL JUN 2014	DRAWN D.J.CAMPBELL JUN 2014	VERIFIED REPQ No. 04340 OCT 2014	RPEQ IAN MUNRO*
		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	SCALE 1:50 AT ORIGINAL SIZE				
SURVEYED		A	CONSTRUCTION ISSUE	DJC								
		B	TRANS LINK AMENDMENTS	DJC								
		C	AMENDED TO SUIT PTIM 2015	MAB	SGM*	JEK*	02/12/2016					
NOTE * Indicates signatures on original issue of drawing or latest revision of the drawing.												




Gladstone Regional Council			5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld. 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au		
GRC			BUS STOP STANDARDS		
BUS STOP - INTERMEDIATE - BLADE SIGN, SHELTER			DETAIL PLAN		

CONSTRUCTION			DRAWING NO.		
GRC-B-004			A3 SHEET		
REVISION			C		

* PATHWAY WIDTH TO BE IN ACCORDANCE WITH GRC ROAD HIERARCHY POLICY.



PLAN


-  WHEEL CHAIR CIRCULATION AREA - 1540 x 2070
-  MINIMUM BOARDING POINT AREA - 1540 x 2070
-  TACTILE GROUND SURFACE INDICATORS

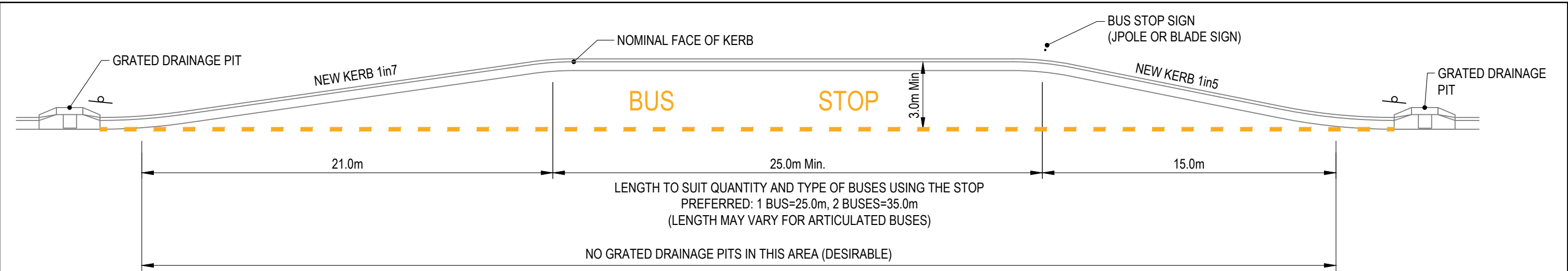


TYPICAL FRONT ELEVATION

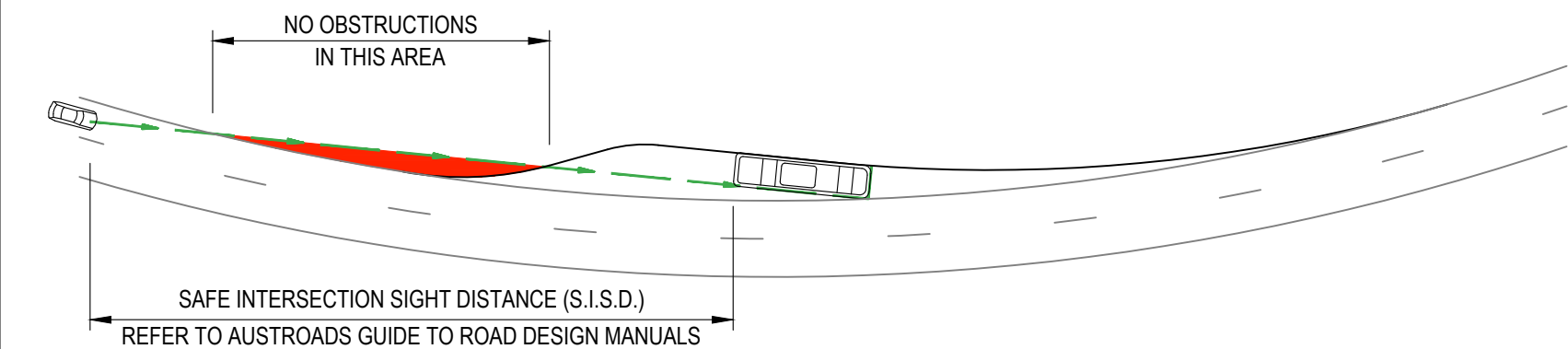
NOTES

- ALL BUS STOPS SHALL BE DSAPT COMPLIANT.
- DIMENSIONS ARE SHOWN IN MILLIMETERS (U.N.O.).
- WHERE PLANTINGS ARE PROVIDED, USE ONLY GROUND COVER OR LOW SHRUBS (<0.5m HIGH). TREES FOR SHADE SHOULD BE LONG-TRUNKED WITH MINIMUM BRANCH HEIGHT OF 4.5m. PLANTINGS SHOULD NOT OBSTRUCT LINE OF SIGHT BETWEEN APPROACHING BUS AND WAITING PASSENGERS. VEGETATION SHOULD BE CLEAR OF ANY EXISTING SERVICES IN THE AREA.
- INSTALL 2 INTERMEDIATE BUS SHELTERS. REFER TO GLADSTONE REGIONAL COUNCIL STANDARD DWGS GRC-B-003 AND GRC-B-004 FOR DETAILS OF SET OUT OF THE SHELTER AND SURROUNDING INFRASTRUCTURE.
- FOR INDENTED BUS BAY DETAILS REFER TO GLADSTONE REGIONAL COUNCIL STANDARD DWG GRC-B-006.
- JOINT SPACING - PLACE A CONTRACTION JOINT EVERY 3m, KEY & EXPANSION JOINTS ARE AT 6m & 18m SPACING RESPECTIVELY.

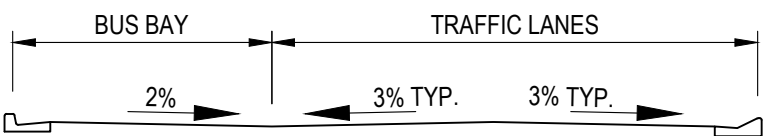
SURVEY REF: COORDS: AZIMUTH DATUM: MGA ZONE 56 LEVEL DATUM: AHDD	RL:	AMENDMENTS						SCALES				GLADSTONE REGIONAL COUNCIL 5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld. 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au	GRC BUS STOP STANDARDS BUS STOP - PREMIUM - 2 SHELTERS, INDENTED BUS BAY DETAIL PLAN	CONSTRUCTION
		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	DESIGNED						
SURVEYED		A	CONSTRUCTION ISSUE	DJC				D.J.CAMPBELL	JUN 2014			GRC-B-005		
		B	TRANSLINK AMENDMENTS	DJC										
		C	GENERAL AMENDMENTS	DJC				D.J.CAMPBELL	JUN 2014			A3 SHEET		
		D	AMENDED TO SUIT PTIM 2015	MAB	SGM*	JEK*	02/12/2016						REVISION	
NOTE * Indicates signatures on original issue of drawing or latest revision of the drawing.								REPQ No.	04340			D		
								RPEQ	IAN MUNRO*					



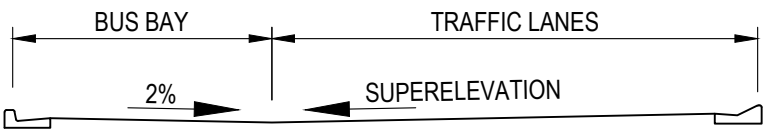
INDENTED BUS BAY
1:200



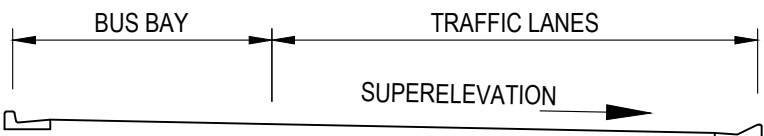
SIGHT DISTANCE ON INSIDE OF CURVE
N.T.S.



STRAIGHT ALIGNMENT



CURVE LEFT




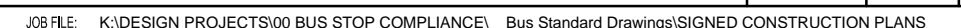
CURVE RIGHT

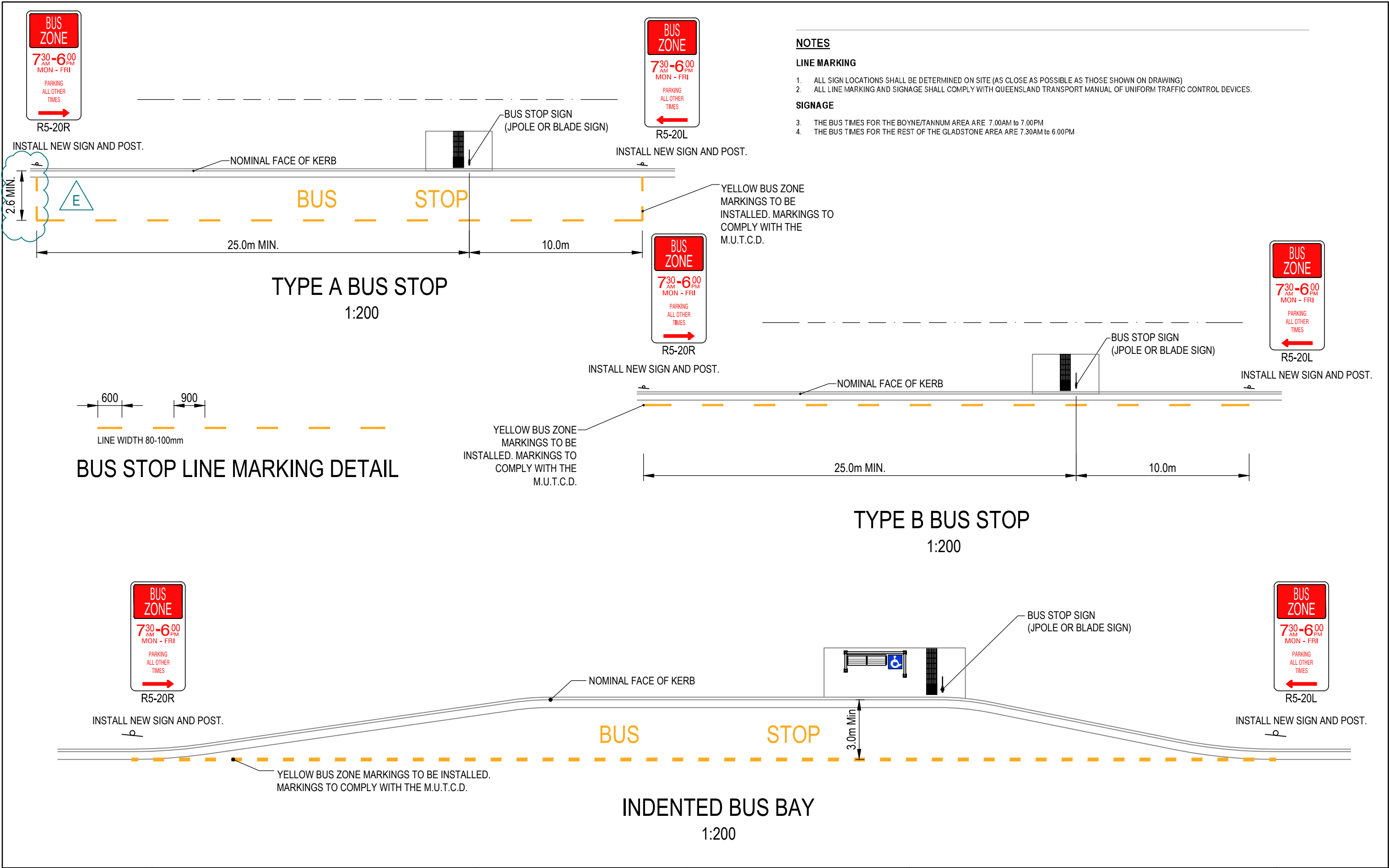
TYPICAL CROSS FALLS
N.T.S.


NOTES

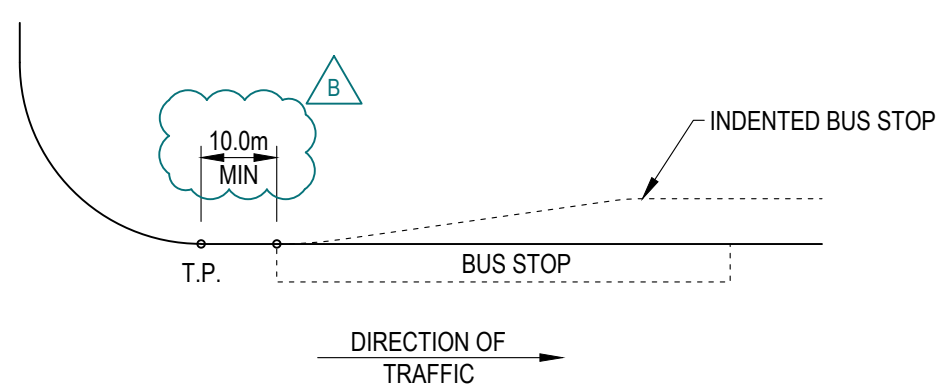
1. THE BUS ZONE SHOULD BE LOCATED SO THAT FOLLOWING DRIVERS HAVE ADEQUATE STOPPING SIGHT DISTANCE TO A BUS EXITING THE BUS ZONE.
2. BUS ZONES SHOULD NOT BE LOCATED ON CURVES. DRIVERS OF BUSES DEPARTING FROM BUS ZONES LOCATED ON LEFT HAND CURVES HAVE PROBLEMS SIGHTING VEHICLES APPROACHING FROM THE REAR DUE TO THE CURVATURE OF THE ALIGNMENT. THEREFORE IT IS NOT DESIRABLE TO LOCATE BUS ZONES ON LEFT CURVE ALIGNMENTS UNLESS IT CAN BE DEMONSTRATED, TO COUNCIL'S SATISFACTION, THAT THIS PROBLEM HAS BEEN OVERCOME WITH MODIFIED BUS ZONE GEOMETRY.
3. THE CROSS FALL ON INDENTED BUS ZONE SHOULD NORMALLY BE 2% TOWARDS THE TRAFFIC LANE. WHERE THE BUS ZONE IS LOCATED ON A RIGHT CURVED SUPERELEVATED ROAD ALIGNMENT, THE BUS ZONE CROSS FALL SHOULD BE THE SAME AS THE TRAFFIC LANES.
4. WHERE AN INVERT RESULTS AT THE EDGE OF THE TRAFFIC LANE, IT IS DESIRABLE THAT GRATED DRAINAGE PITS ARE NOT LOCATED WITHIN THE LENGTH OF THE BUS ZONE.
5. REFER TO THE LATEST AUSTROADS GUIDE TO ROAD DESIGN MANUALS.

SURVEY REF: COORDS: AZIMUTH DATUM: MGA ZONE 56 LEVEL DATUM: AHDD	RL:	AMENDMENTS					SCALES			GLADSTONE REGIONAL COUNCIL  5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld. 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au	GRC BUS STOP STANDARDS INDENTED BUS BAY SETOUT PLAN	CONSTRUCTION			
		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	Scales As shown. (UNO) FULL SIZE A3				DRAWING NO.			
		A	CONSTRUCTION ISSUE	DJC				DESIGNED	D.J.CAMPBELL	JUN 2014					
								DRAWN	D.J.CAMPBELL	JUN 2014					
								VERIFIED							
								REPQ No.	04340	7/10/2014					
								RPEQ	IAN MUNRO*						
SURVEYED															
NOTE * Indicates signatures on original issue of drawing or latest revision of the drawing.															

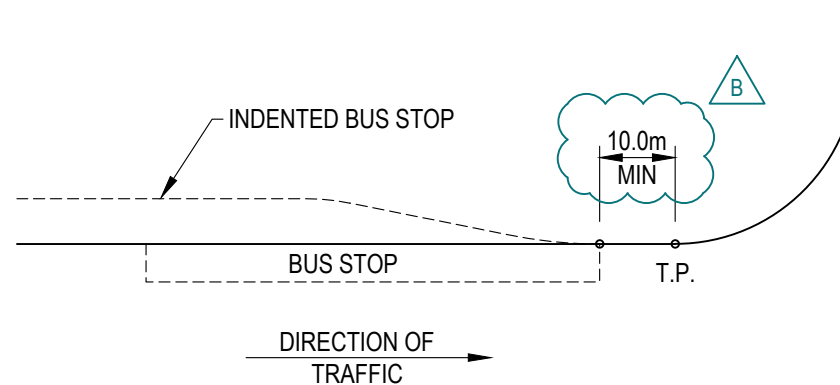




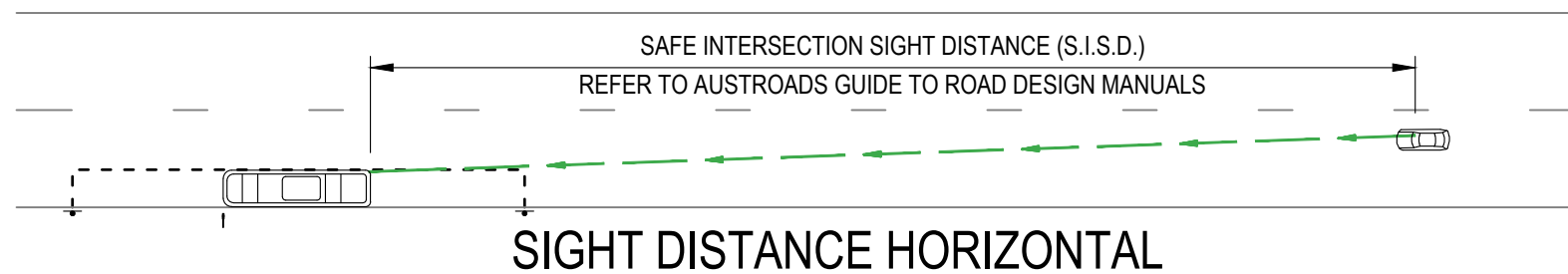
SURVEY REF: COORDS: AZIMUTH DATUM: MGA ZONE 56 LEVEL DATUM: AHDD	RL:	AMENDMENTS						SCALES			<div>GLADSTONE REGIONAL COUNCIL</div> <div></div> <div>5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld, 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au</div>	<div>GRC</div> <div>BUS STOP STANDARDS</div> <div>BUS STOP LINE MARKING</div> <div>DETAIL PLAN</div>			CONSTRUCTION		
		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	Drawing No.									
SURVEYED		A	CONSTRUCTION ISSUE	DJC				DESIGNED	D.J.CAMPBELL	JUN 2014		GRC-B-009					
		B	TRANSLINK AMENDMENTS	DJC				DRAWN	D.J.CAMPBELL	JUN 2014		A3 SHEET					
		C	NOTE AMENDED	DJC				VERIFIED				REVISION					
NOTE * Indicates signatures on original issue of drawing or latest revision of the drawing.		D	AMENDED TO SUIT PTIM 2015	MAB				REPQ No.	04340	OCT 2014							
		E	BUS BAY WIDTH AMENDED	DJC	SGM*	JEK*	02/12/2016	RPEQ	IAN MUNRO*								



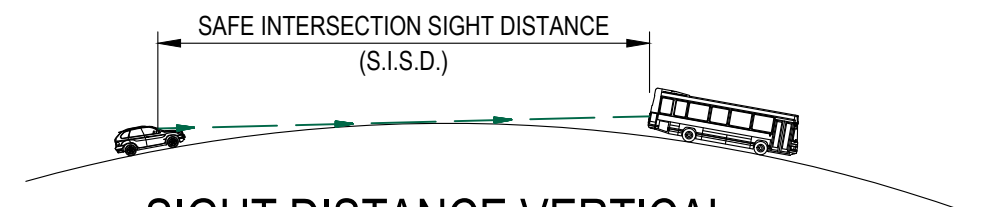
DEPARTURE SIDE OF INTERSECTION



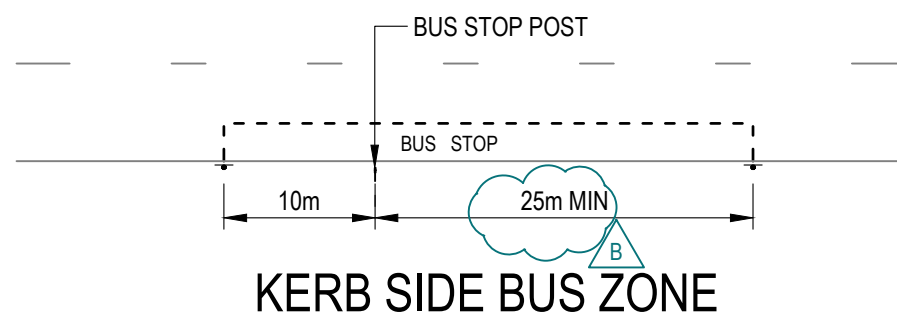
APPROACH SIDE OF INTERSECTION



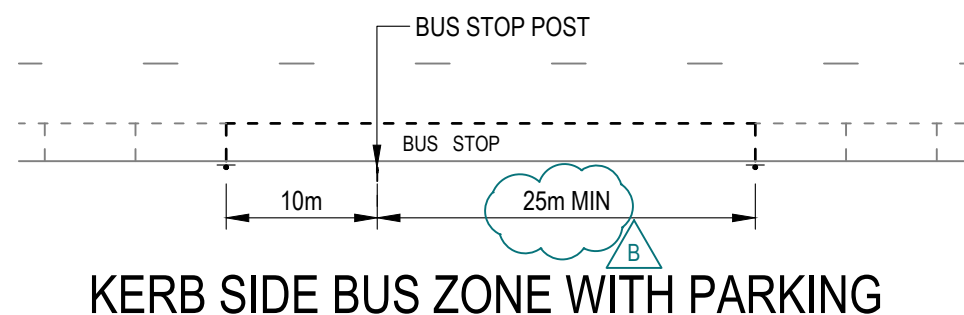
SIGHT DISTANCE HORIZONTAL



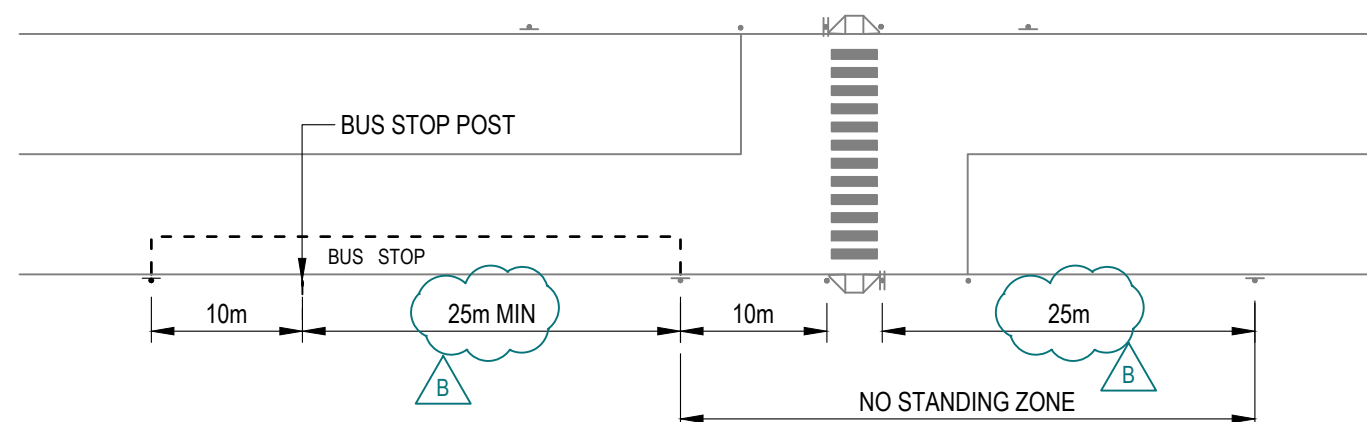
SIGHT DISTANCE VERTICAL



KERB SIDE BUS ZONE



KERB SIDE BUS ZONE WITH PARKING

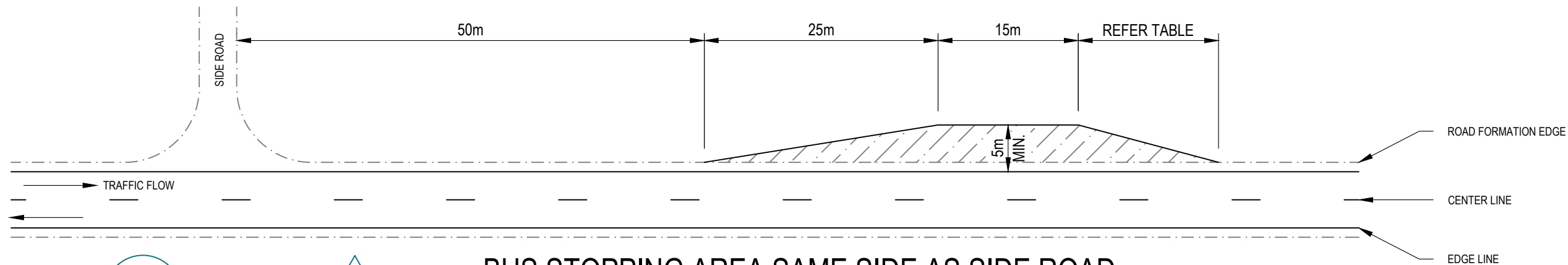


KERB SIDE BUS ZONE NEAR CROSSING

NOTES

- BUS ZONES NEAR PEDESTRIAN CROSSINGS SHOULD BE LOCATED ON THE DEPARTURE SIDE. PASSENGERS ARE NOT OBSCURED FROM VIEW BY THE BUS, AND ARE READILY SEEN BY APPROACHING DRIVERS.
- A BUS ZONE THAT IS REQUIRED TO BE IN CLOSE PROXIMITY TO AN INTERSECTION SHOULD BE PLACED ON THE DEPARTURE SIDE OF THE INTERSECTION. ALTERNATIVELY, THE BUS ZONE CAN BE PLACED IN ADVANCE OF THE INTERSECTION IF THE TYPE/ORIGIN/DESTINATION OF THE PASSENGERS USING THE SERVICE WARRANTS THIS.
- BUS ZONES SHOULD NOT BE COMBINED WITH ACCELERATION OR DECELERATION LANES.
- THE BUS ZONE SHOULD BE LOCATED SO THAT FOLLOWING DRIVERS HAVE ADEQUATE STOPPING SIGHT DISTANCE TO A BUS EXITING THE BUS ZONE.
- BUS ZONES SHOULD NOT BE LOCATED ON CURVES. DRIVERS OF BUSES DEPARTING FROM BUS ZONES LOCATED ON LEFT HAND CURVES HAVE PROBLEMS SIGHTING VEHICLES APPROACHING FROM THE REAR DUE TO THE CURVATURE OF THE ALIGNMENT. THEREFORE IT IS NOT DESIRABLE TO LOCATE BUS ZONES ON LEFT CURVE ALIGNMENTS UNLESS IT CAN BE DEMONSTRATED, TO COUNCIL'S SATISFACTION, THAT THIS PROBLEM HAS BEEN OVERCOME WITH MODIFIED BUS ZONE GEOMETRY.
- WHERE AN INVERT RESULTS AT THE EDGE OF THE TRAFFIC LANE, IT IS DESIRABLE THAT GRATED DRAINAGE PITS ARE NOT LOCATED WITHIN THE LENGTH OF THE BUS ZONE.
- REFER TO THE LATEST
 - AUSTRROADS GUIDE TO ROAD DESIGN MANUALS.

SURVEY REF: COORDS: AZIMUTH DATUM: MGA ZONE 56 LEVEL DATUM: AHDD	RL:	AMENDMENTS					SCALES			<div><div></div><div>5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld. 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au</div></div>	<div><div><div>G</div></div><div><div>GRC</div><div>BUS STOP STANDARDS</div><div>KERB SIDE BUS ZONE</div><div>SETOUT DETAILS</div></div></div>	<div><div>CONSTRUCTION</div><div>DRAWING NO.</div><div>GRC-B-010</div><div>A3 SHEET</div><div>REVISION</div><div>B</div></div>	
		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	Not to Scale. (UNO) FULL SIZE A3					
		A	CONSTRUCTION ISSUE	DJC				DESIGNED	D.J.CAMPBELL				JUN 2014
		B	AMENDED TO SUIT PTIM 2015	MAB	SGM*	JEK*	25/10/2016	DRAWN	D.J.CAMPBELL				JUN 2014
SURVEYED							VERIFIED						
NOTE * Indicates signatures on original issue of drawing or latest revision of the drawing.							REPQ No.	04340	7/10/2014				
							RPEQ	IAN MUNRO*					



PREFERRED TAPER LENGTHS

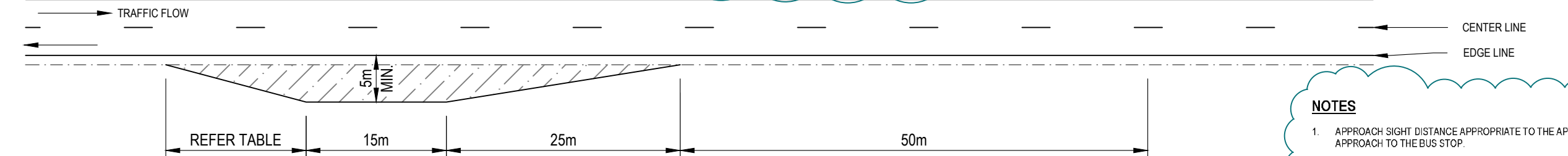
SPEED km/h	m (MIN)
60	25
80	50
100	75



GRAVEL SUB BASE AT EXISTING CROSS FALL
BITUMEN SEAL (OPTIONAL).
FOR DESIGN PAVEMENT DETAILS REFER NOTE 5

2-COAT SEAL (INDICATIVE DESIGN)
TOP COAT: C170 BITUMEN @ 1.9 l/m²
14MM AGGREGATE @ 95m²/m³
BOTTOM COAT: C170 BITUMEN @ 1.1 l/m²
10MM AGGREGATE @ 140m²/m³

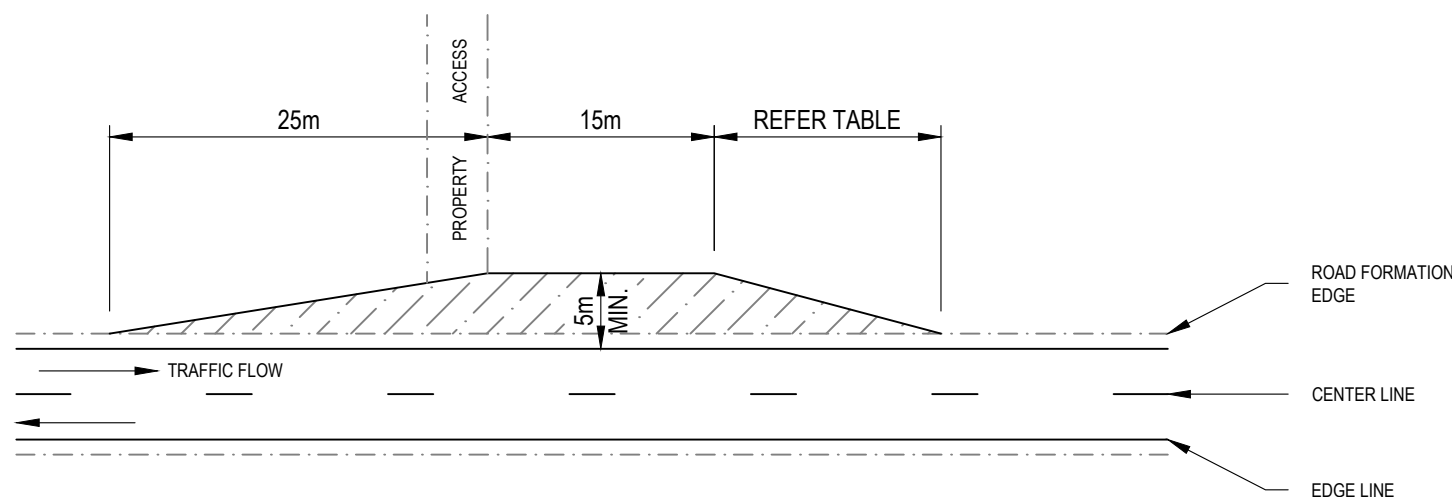
BUS STOPPING AREA SAME SIDE AS SIDE ROAD



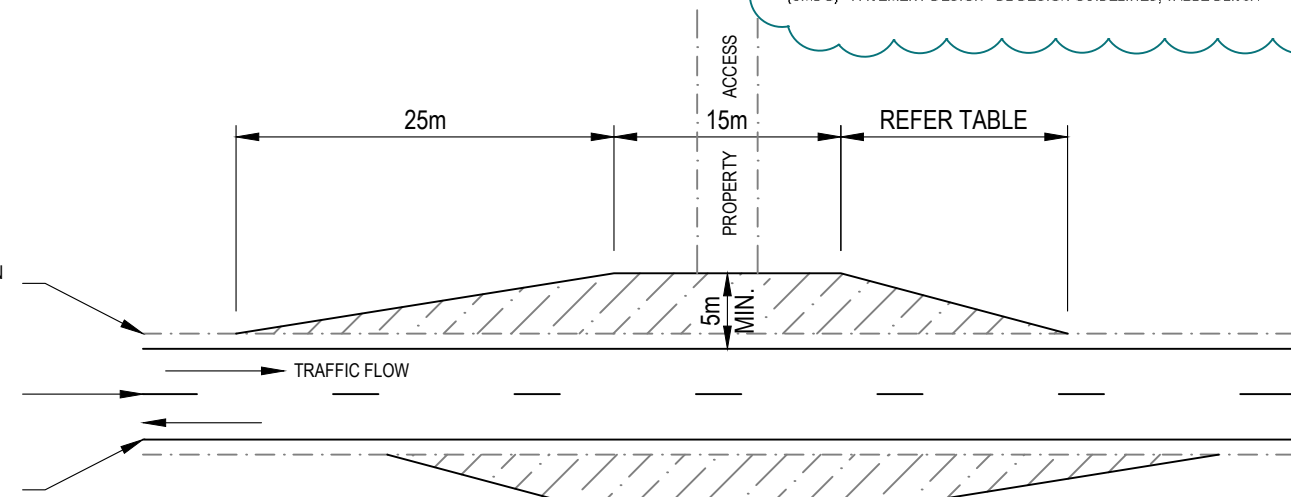
BUS STOPPING AREA OPPOSITE SIDE TO SIDE ROAD

NOTES


1. APPROACH SIGHT DISTANCE APPROPRIATE TO THE APPROACH SPEED SHOULD BE PROVIDED ON EACH APPROACH TO THE BUS STOP.
2. A SCHOOL BUS STOPPING LOCATION SHOULD ONLY BE SIGNED AS A SCHOOL BUS STOP WHERE SIGHT DISTANCE IS RESTRICTED AND CANNOT BE SUFFICIENTLY IMPROVED.
3. THE SCHOOL BUS STOP SHOULD BE LOCATED ON THE DEPARTURE SIDE OF INTERSECTIONS, CHILDREN'S CROSSING LOCATIONS, AND PROPERTY ACCESSES, WHERE POSSIBLE.
4. REFER TO THE LATEST
 - AUSTROADS GUIDE TO ROAD DESIGN MANUALS.
 - DEPARTMENT OF TRANSPORT & MAIN ROADS GUIDE FOR THE ROAD SAFETY MANAGEMENT OF RURAL SCHOOL BUS ROUTES AND BUS STOPS.
5. FOR PAVEMENT DEPTHS AND DETAILS REFER TO CAPRICORN MUNICIPAL DEVELOPMENT GUIDELINES (CMDG) - PAVEMENT DESIGN - D2 DESIGN GUIDELINES, TABLE D2.08.1



BUS STOPPING AREA ADJACENT TO PROPERTY ACCESS



BUS STOPPING AREA ACROSS AND/OR OPPOSITE PROPERTY ACCESS

SURVEY REF: COORDS: AZIMUTH DATUM: MGA ZONE 56 LEVEL DATUM: AHDD	RL:	AMENDMENTS					SCALES			 <div>5 Don Cameron Drive, Calliope Qld 4680 P.O. Box 29, Gladstone DC Qld. 4680 Ph. +61 7 49700700 Fax. +61 7 49758500 Email: DesignServices@gladstonerc.qld.gov.au</div>	<div>GRC BUS STOP STANDARDS RURAL SCHOOL BUS STOP AREA SETOUT PLAN</div>		CONSTRUCTION		
		REV	DESCRIPTION	DRAWN	VERIFIED	APPROVED	DATE	Scales As shown. (UNO) FULL SIZE A3							
		A	CONSTRUCTION ISSUE	DJC				DESIGNED	D.J.CAMPBELL				JUN 2014	GRC-B-011	
		B	NOTES AMENDED	DJC	SGM*	JEK*	02/12/2016	DRAWN	D.J.CAMPBELL				JUN 2014		
SURVEYED							VERIFIED			REPQ No.	04340				
NOTE * Indicates signatures on original issue of drawing or latest revision of the drawing.									RPEQ			A3 SHEET	REVISION	B	